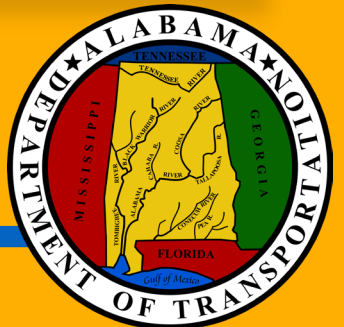


# ALDOT Statewide Freight Plan Update

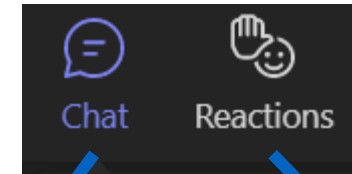
*Freight Advisory Committee Meeting #3*

*April 27, 2023*



# Teams Meeting

- Please keep your computer audio or phones on “mute”.
- If you would like to ask a question or provide a comment, click “Reactions” to raise your hand or you may unmute yourself.
- You may also ask questions or enter comments in the Chat window, and we will pause periodically during the presentation to address questions or comments.



Click “Chat” to ask a question or leave a comment

Click “Reactions” to raise your hand, or simply unmute yourself to speak

# Agenda

- Federal Freight Planning Guidance
- Truck Parking Assessment
- Significant Freight Investments
- Planning for the First and Last Mile
- Discussion

# Federal Freight Planning Guidance



# Statewide Freight Plans

- 21 states with approved, BIL-compliant plans
- Ohio, Virginia, Iowa, Nevada, Rhode Island, Wyoming, Vermont, Missouri, Maryland, Pennsylvania, Washington, Idaho, North Dakota, Arizona, Alaska, Mississippi, Connecticut, Oklahoma, Delaware, South Carolina, New Mexico, Montana

United States Department of Transportation  
Federal Highway Administration

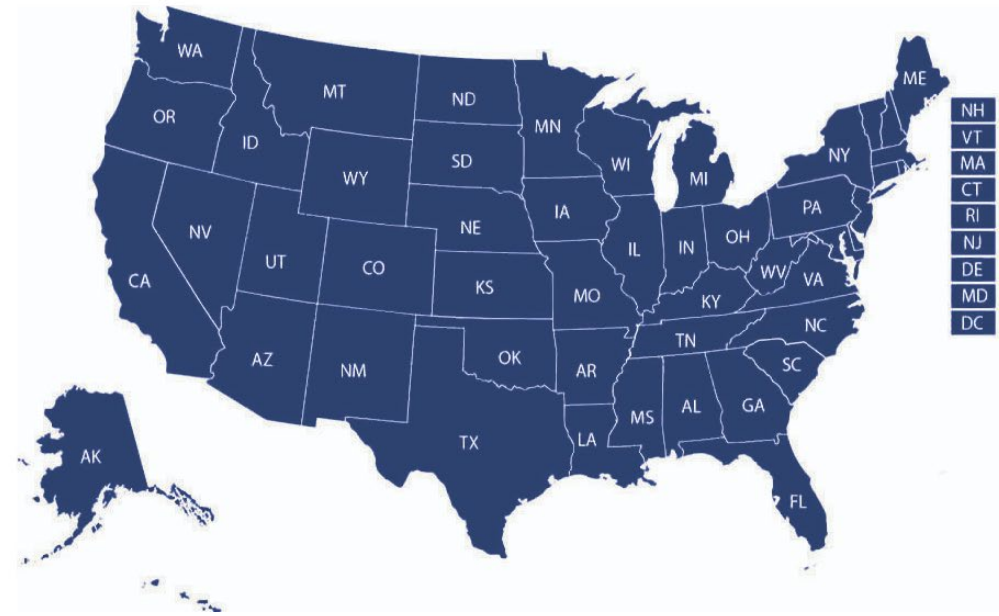
OFFICE OF OPERATIONS: Freight Management and Operations  
**FREIGHT PROFESSIONAL CAPACITY BUILDING PROGRAM**

HOME PEER ASSISTANCE NOTEWORTHY PRACTICES EDUCATION & TRAINING DATA & TOOLS - PLANS & PROGRAM AREAS -

STATE FREIGHT PLANNING TOOLKIT KEY FACTORS STRATEGIES & PERFORMANCE MEASURES ALL PLANS

**BROWSE ALL CURRENT STATE FREIGHT PLANS**

The FHWA monitors and approves all State Freight Plan updates to ensure compliance with current federal surface transportation laws. Click on a State below to view their most recent State Freight Plan.



# New Statewide Freight Plan Requirements

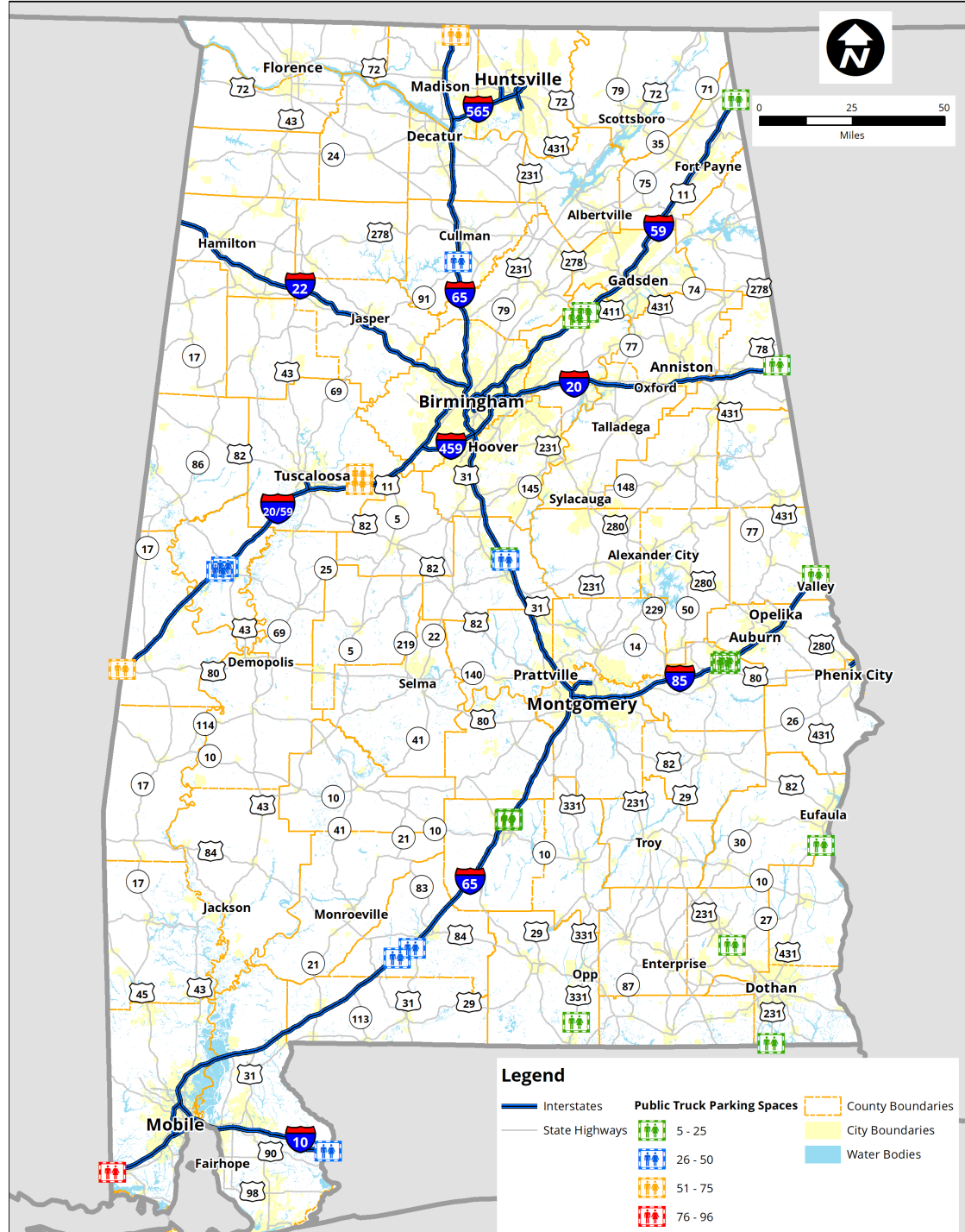
- Updates now required every 4 years (instead of 5 years)
- New Multimodal Freight Office oversees and provides guidance to State DOTs
- New Required Elements in Freight Plans:
  - Truck parking assessment
  - Supply chain cargo flows by mode
  - Commercial ports
  - Impacts of e-commerce
  - Military freight
  - Multi-state freight compacts (not applicable to AL)
  - Strategies for extreme weather, natural disasters, air pollution, flooding and stormwater runoff, and wildlife habitat loss

# Truck Parking Assessment

# Truck Parking Assessment

- Hours-of-service regulations and limited truck parking availability can make it challenging to find truck parking – can lead to unauthorized parking on off-ramps or vacant lots
- MAP-21 included “Jason’s Law” - requires USDOT to assess and address capability of each state to provide adequate truck parking and rest facilities
- IIJA requires State Freight Plans to address truck parking needs





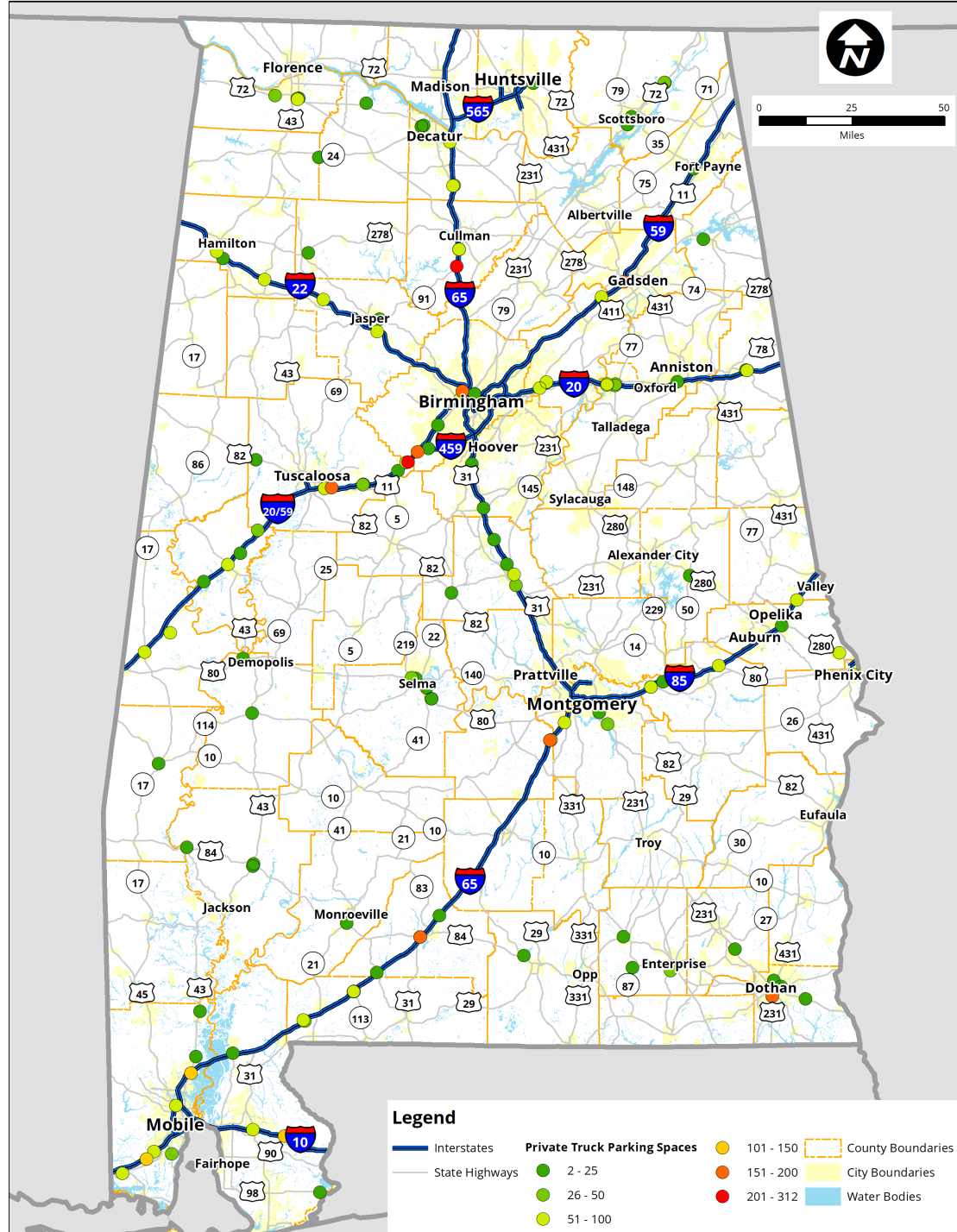
# Parking Inventory: Public Facilities

- Public truck parking spaces are located in rest areas and welcome centers along interstates
- Greatest number of spaces at welcome center along I-10 eastbound in Grand Bay
- No public truck parking spaces along I-22 between Mississippi and Birmingham



# Parking Inventory: Private Facilities

- Private truck parking spaces located at truck stops/large gas stations, along interstates and other highways
- Clusters west and south of Birmingham and west of Mobile
- Limited private truck parking on I-59 northeast of Birmingham and I-65 south of Montgomery



# Truck Parking Demand

- FHWA Truck Parking Methodology
- Estimated current truck parking needs (2022) and projected future truck parking needs (2050) based on truck volumes, posted speed limit, and peak parking factors for long-haul and short-haul truck trips

**11,436**

Truck Parking Spaces on Interstates

**75%**

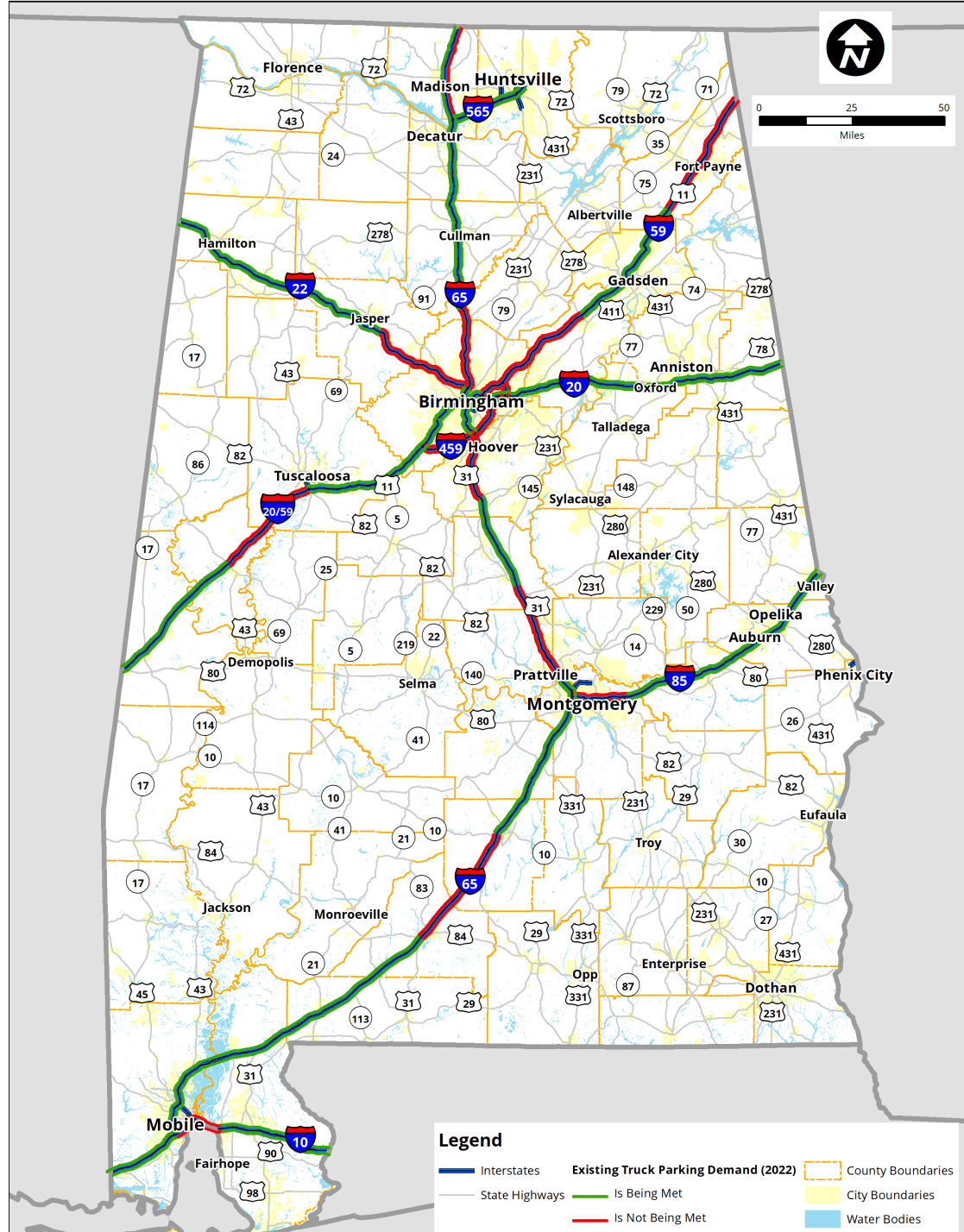
Statewide Truck Parking Demand Increase by 2050

**102.5%**

Parking Demand Increase I-65 NB between US 43 (Creola) & US 84 (Evergreen)



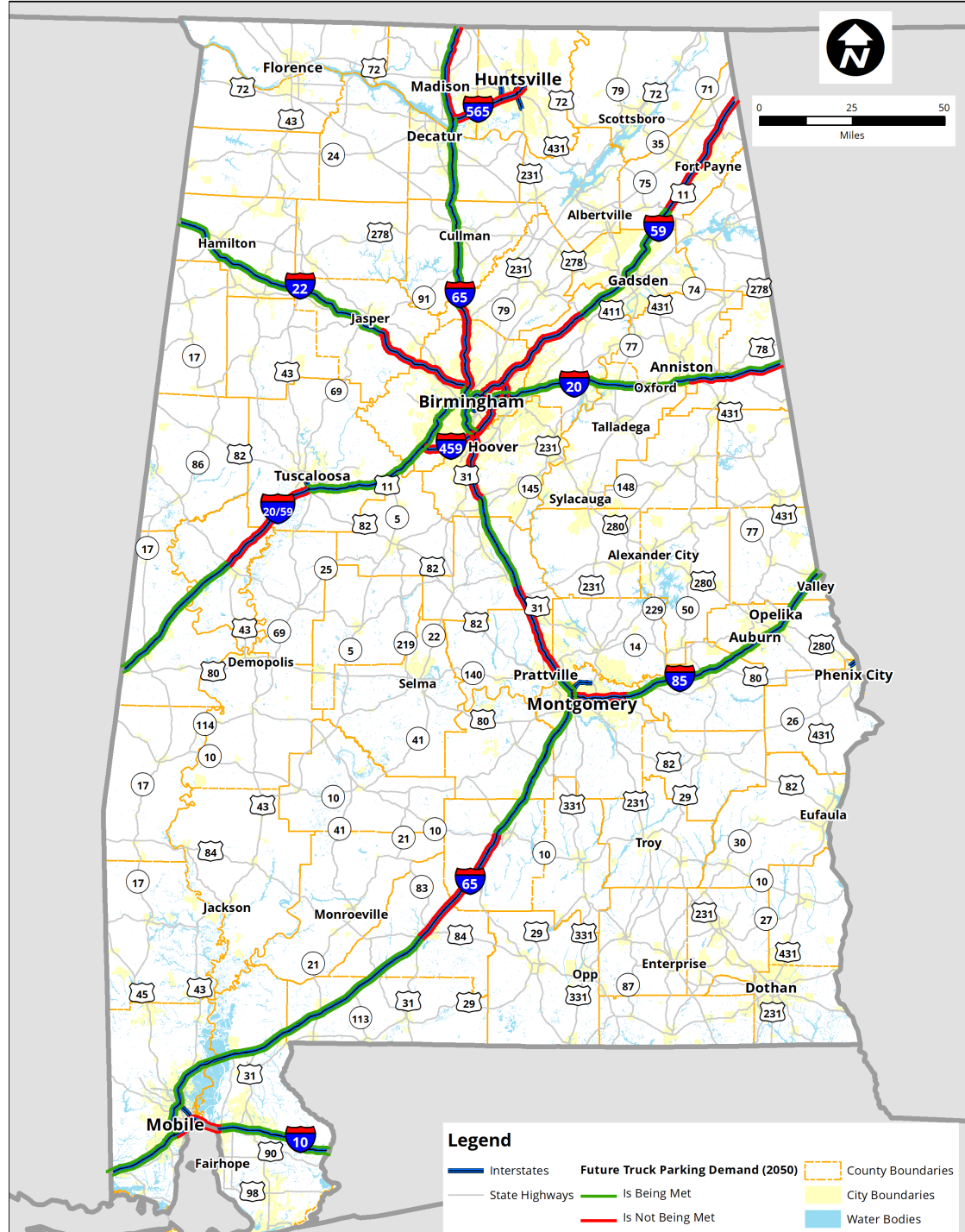
Corridor with Highest Truck Parking Deficit by 2050



# Existing Truck Parking Demand (2022)

- Most segments of interstates in Alabama have adequate truck parking
- More parking needed along I-20/I-59, I-59 and I-65 near the state line, I-65 north and south of Montgomery, and around Birmingham





# Projected Truck Parking Demand (2050)

- In the future, most segments of interstates in Alabama projected to have adequate truck parking
- Higher parking needs are projected along segments where there is already a need (75% increase in demand statewide)
- Future truck parking needs on I-20 EB near Georgia state line, and I-565 near Huntsville

# Significant Freight Investments



# Birmingham Central Business District (CBD)

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- Bridge replacements at I-20/  
I-59 interchange in Downtown  
Birmingham
- One of the worst bottlenecks  
for trucks in the state
  - Over 29,000 hours of truck  
delay per mile in 2017
  - After project, less than  
4,900 hours of truck delay  
per mile in 2022

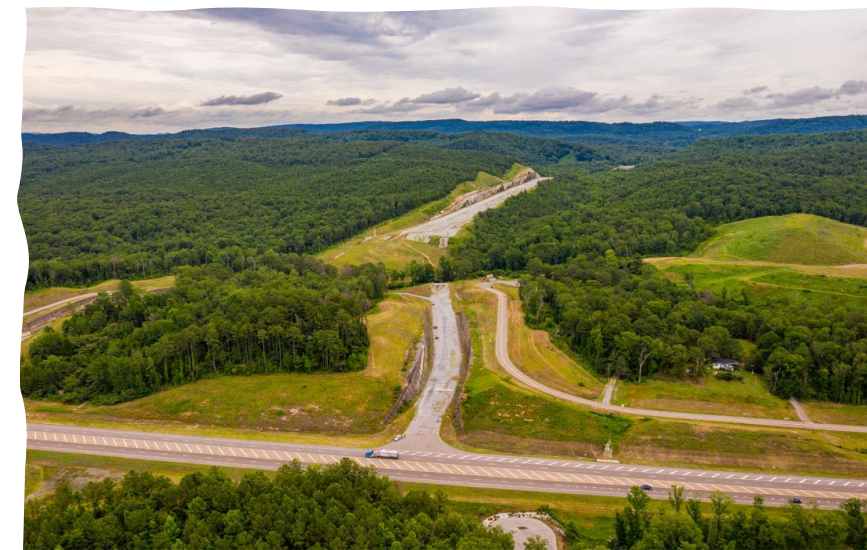
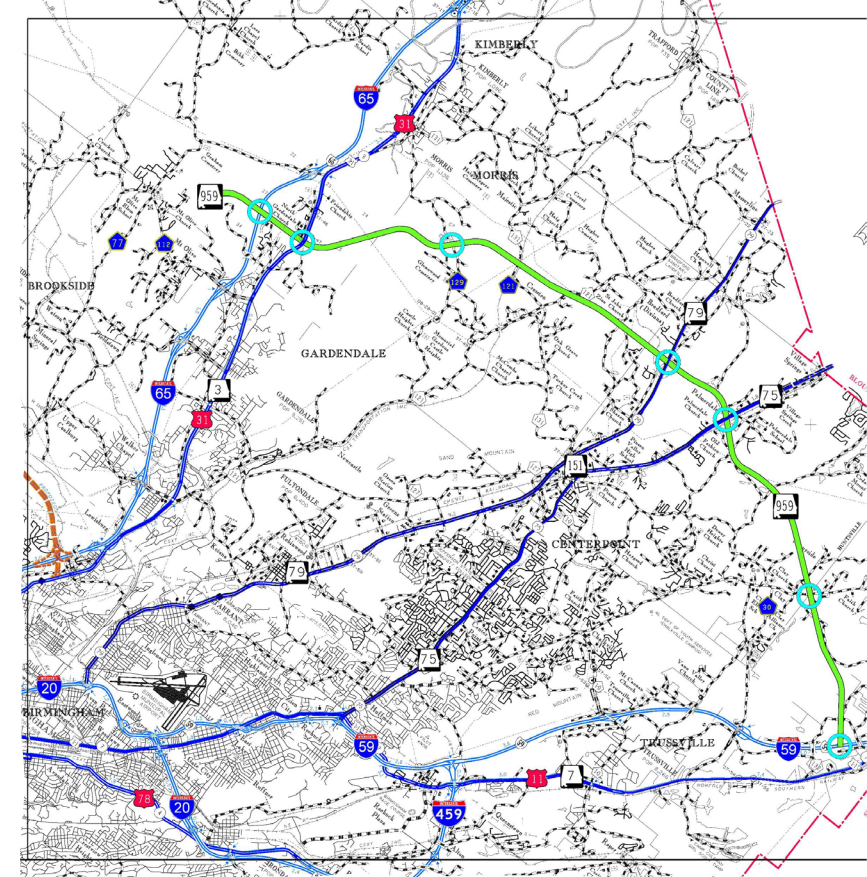




# Northern Beltline Project

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- 52-mile corridor from I-59 in northeast Jefferson County to the I-459 interchange with I-59/20 near Bessemer
- Will serve as an alternate route for a projected 15,000 trucks per day
- Will reduce congestion and improve safety on interstates regionwide
- Expected to spur economic development



# Mobile River Bridge and Bayway

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- New main span signature bridge between Mobile and Daphne with seven new interchanges
- Increased capacity will address truck bottleneck
- Both toll and toll-free routes (toll revenue to help pay down project debt)
- Funded through prior INFRA grant as well as state funding; ALDOT seeking additional federal funds







Source: Google Earth

## I-85 Bridge Widenings

- Widening bridges on I-85 between Montgomery and Auburn and towards Georgia state line
- Critical corridor for freight traffic to and from Georgia and the East Coast
- Maintenance and safety projects will improve mobility and safety for trucks and all vehicles on this part of I-85
- Construction will continue for the next 5-7 years



# Inland Intermodal Transfer Facility

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- New inland intermodal transfer facility on 272 acres in Montgomery County
- Will extend CSX rail service from Port of Mobile to Port of Montgomery
- Expected to generate over 2,600 jobs, \$340 million in business revenue, and \$14 million+ in local and state taxes
- First phase to be constructed over two years

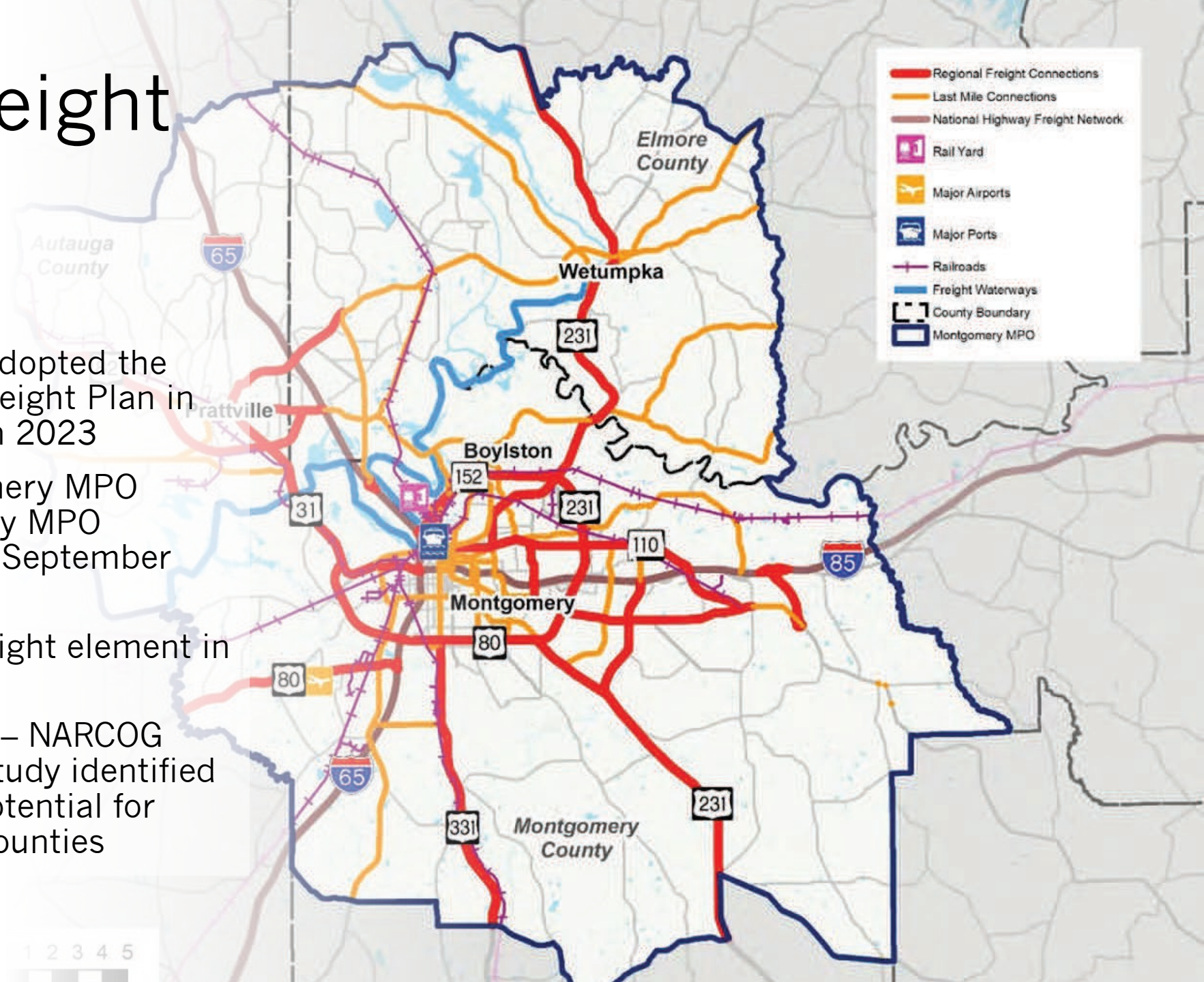


# Planning for the First and Last Mile



# Regional Freight Planning in Alabama

- **Birmingham** – RPCGB adopted the Birmingham Regional Freight Plan in 2019 and RTP update in 2023
- **Montgomery** – Montgomery MPO adopted the Montgomery MPO Regional Freight Plan in September 2020
- **Mobile** – Very robust freight element in RTP
- **North-Central Alabama** – NARCOG Inland Port Feasibility Study identified commodity flows and potential for industrial sites in four counties



# Potential Funding Sources for Freight Improvements

## **Formula Funds**

- National Highway System (NHS) Funds
- Surface Transportation Block Grant (STBG) Funds – Urban
- Promoting, Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT)
- National Electric Vehicle (EV) Formula Program



# Potential Funding Sources for Freight Improvements

## **Discretionary Funds**

- Promoting, Resilient Operations for Transformative, Efficient, and Cost-saving Transportation (PROTECT)
- National Electric Vehicle (EV) Formula Program
- Charging and Fueling Infrastructure Program
- Consolidated Rail Infrastructure and Safety Improvement (CRISI) Grants - FRA
- Railroad Crossing Elimination Grant - FRA





# Applications of the Statewide Freight Plan

- Provides a policy framework that can serve as a basis for MPO plans
- Identifies the types of businesses that are freight-reliant
- Identifies significant roadways and industries in a specific region
- Identifies specific bottlenecks that require more detailed attention
- Identifies specific corridors that are underserved by truck parking

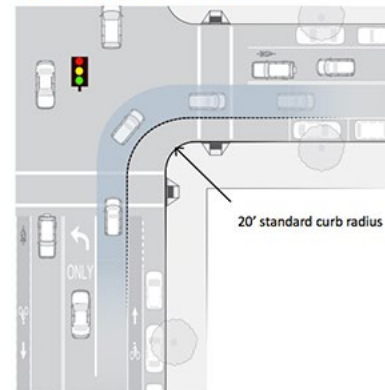


# Catalysts for First/Last Mile Improvements

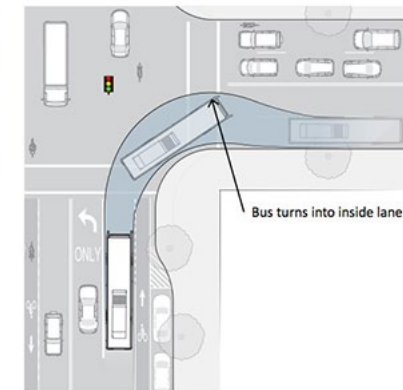
- New industrial development along roads not originally designed to accommodate large commercial vehicles
- Need to revive and redevelop older industrial districts
- Changes in freight vehicles and new freight technology



Typical Curb Radius -  
Signalized Intersection



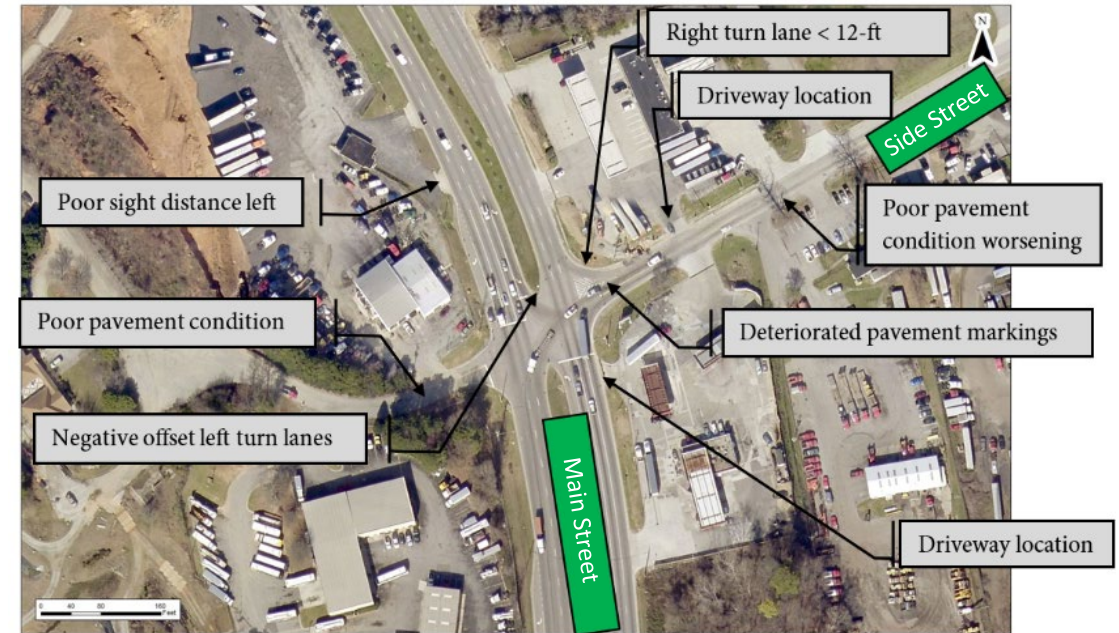
4-lane Signalized Intersection





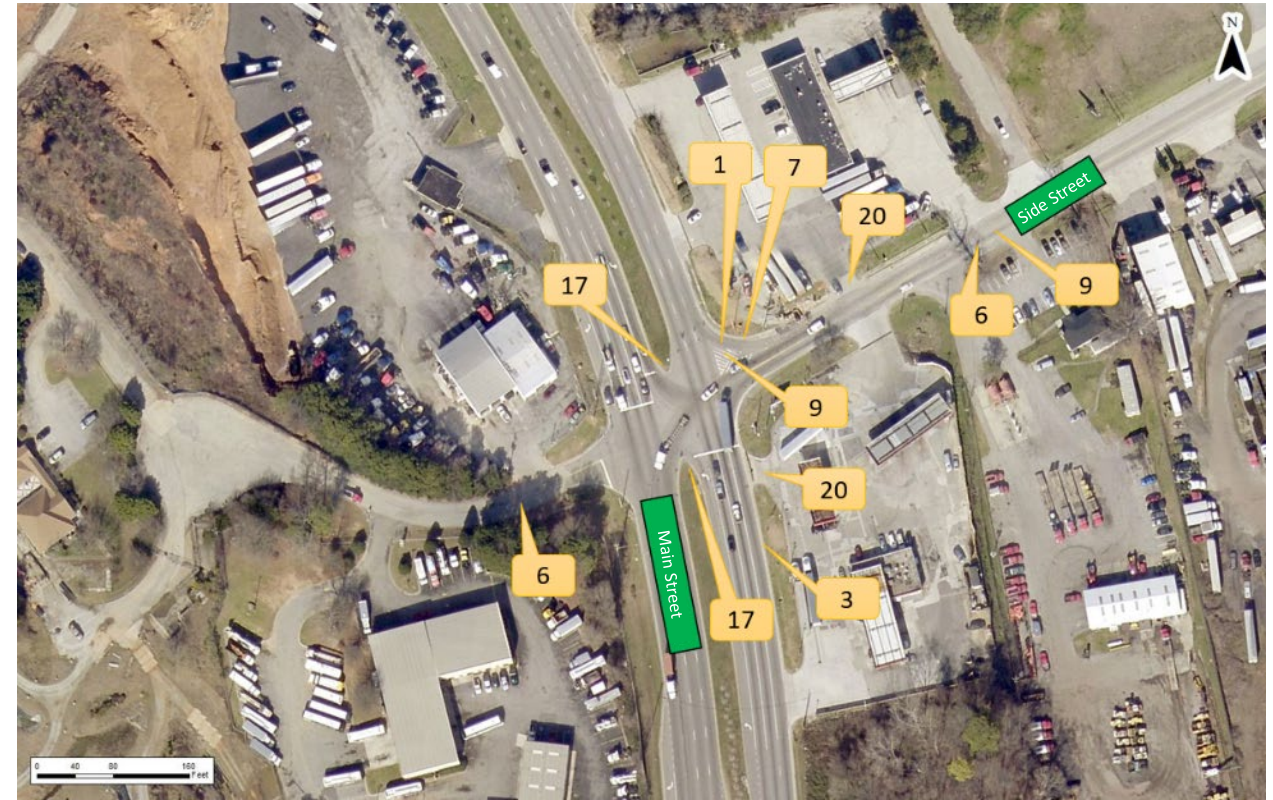
# Indicators for Local Freight Improvements

- Commercial vehicle crashes
- Long queues at signals due to truck acceleration times
- Tire markings at intersection approaches
- Damage to median noses and sidewalks



# Types of Local Freight Improvements

- Intersection reconfiguration
- Driveway consolidation
- Signal enhancements
- ITS enhancements (CV/AV)



Typical Proposed Improvements							
1	Widening	6	Pavement	11	Paved Shoulder	16	U-Turn Bulb-Out
2	Decel/Accel Lane	7	Turning Radius	12	Lighting	17	Median end-treatments
3	Dedicated Right Turn	8	Pedestrian Facilities	13	Roadway signage	18	Opposing left-turn lanes
4	Dedicated Left Turn	9	Pavement Markings	14	Drainage	19	Roundabout
5	Lateral Offset	10	Traffic Signal	15	Storage Lane	20	Driveway closure

# Discussion




# Questions

- How is your region or business planning for freight?
- Do you have any comments or questions about the Draft Statewide Freight Plan?



**2022 Alabama  
Statewide Freight Plan**  
February 2023



Prepared by



# Next Steps

- Addressing final comments from FHWA
- Review feedback from FAC
- Final approval and adoption by FHWA and ALDOT

# Thank You!

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